

LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date : 16th February 2011

Report of
Assistant Director, Planning &
Environmental Protection

Contact Officer:
Aled Richards Tel: 020 8379 3857
Andy Higham Tel: 020 8379 3848
Mrs S.L. Davidson Tel: 020 8379
3841

Ward: Grange

Application Number : TP/10/0473

Category: Dwellings

LOCATION: 1, CRESCENT ROAD and 33, WAVERLEY ROAD, ENFIELD, EN2 7BN

PROPOSAL: Redevelopment of site to provide a 3-storey block of 9 self contained flats (comprising 7 x 2-bed and 2 x 4-bed) involving accommodation in roof with dormer windows to front and side and a roof terrace, balconies to all sides and basement parking with access to Crescent Road.

Applicant Name & Address:

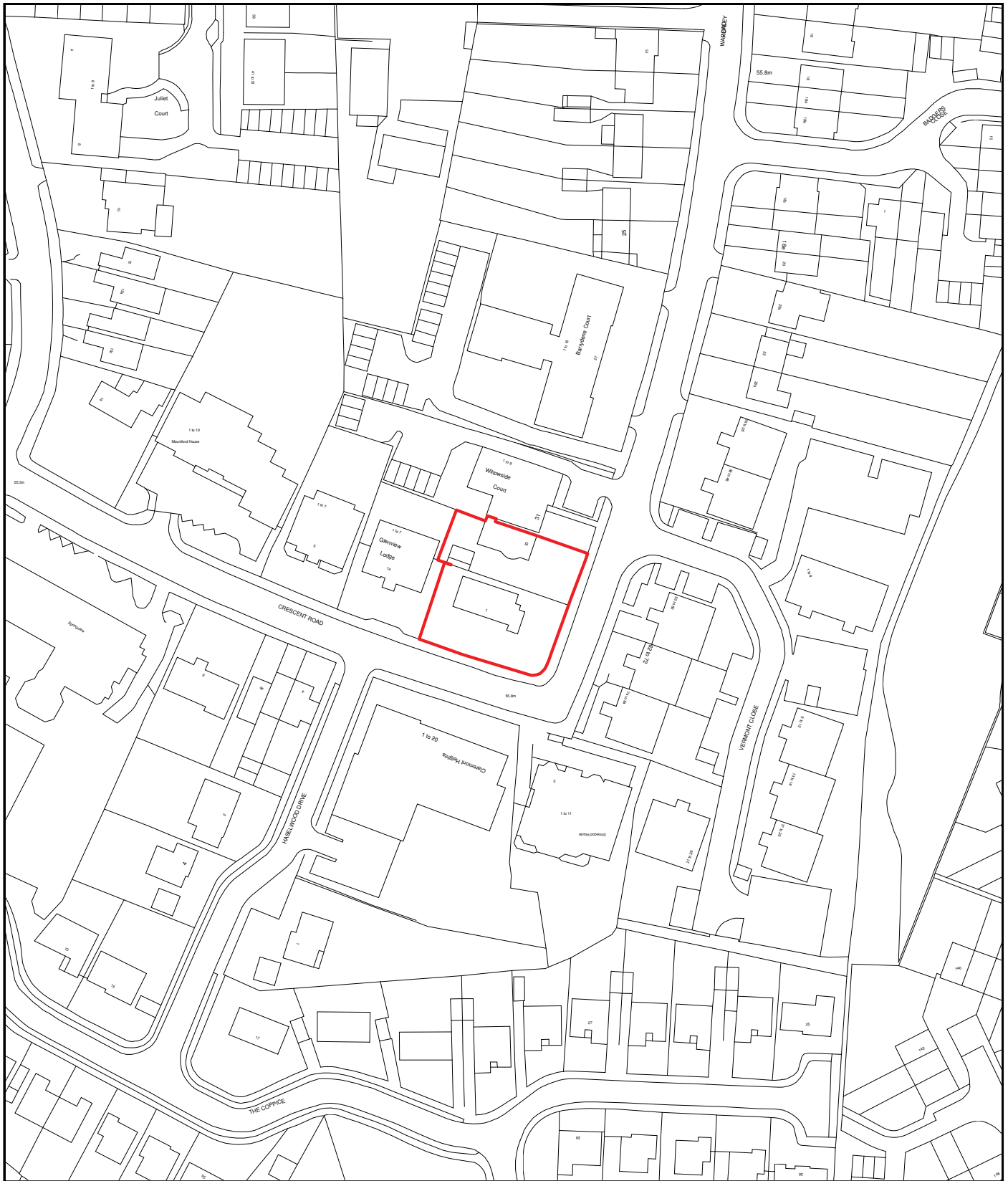
Mr L Hava
c/o agent

Agent Name & Address:

Mr Ian Coward,
Collins & Coward
Westwood Park
London Road
Little Horkesley
Colchester
Essex
CO6 4BS

RECOMMENDATION: That subject to the satisfactory completion of a S106 Agreement to secure the contribution referred to in this report, planning permission be **GRANTED** subject to conditions.

Application No:- TP/10/0473



Development Control



Scale: 1:1250
Time of plot: 11:07

Date of plot: 04/02/2011

Note for Members

This application was reported to the 16th December 2010 Planning Committee when Members resolved to defer consideration to enable officers to enter into further discussions with the applicant to increase the provision of amenity space.

The scheme previously considered achieved a level of amenity space equivalent to 61% of the gross internal area of the building. Amendments have now been made to the scheme, including the removal of one of the visitor parking spaces to the Crescent Road frontage and an increase in the size of the roof terrace, so that amenity space provision now equates to 73% of the gross internal area of the building.

It would be possible to further increase amenity space provision by removing the remaining visitor parking space, without compromising the ability of the scheme to meet the London Plan parking standards. If considered necessary, this would result in a level of amenity space of 76%, in excess of the Council's minimum standards. However, on balance, it is considered beneficial to maintain a surface level visitor space and therefore this option has not been pursued

With the loss of one visitor parking space, the scheme still makes provision for a total of 12 car parking spaces to support 9 flats proposed; a ratio of 1.3 spaces per unit. This still exceeds London Plan standards.

Consultation has taken place on the revised plans in respect of the removal of the frontage car parking spaces. Any further observations received will be reported at the meeting.

In the light of the increased level of amenity space provision, the contribution to off-site open space enhancements previously referred to is no longer recommended.

Since the application was reported to Planning Committee in December one further letter of objection has been received from an occupier within the adjacent block of flats at Willowside Court. The objections raised can be summarised as:

- the development would restrict light to the bathroom windows in the flank elevation of this block
- the development would restrict light to bedrooms in the rear elevation of the block
- the sunny aspect already ruined by the building of Glenview in Crescent Road
- increasing density leads to increase traffic

1 Site and Surroundings

1.1 The application site is located at the corner of Crescent Road and Waverley Road and is presently occupied by two detached chalet style bungalows. The site is one of the few undeveloped plots in the immediate vicinity and is surrounded on all sides by more recent flatted redevelopment schemes. The site immediately to the north, Willowside Court, comprises a three storey

block of flats; to the west the site adjoins Glenview Lodge, a four storey block with basement car parking. Opposite the site are Claremont Heights, Elmwood House and Nos. 50-84 Waverley Road, flatted developments of similar scale.

- 1.2 The application site contains two trees the subject of a Tree Preservation Order, a Copper Beech within the front garden of No.33 Waverley Road and a Sweet Chestnut within the front garden of No. 1 Crescent Road.

2 Proposal

- 2.1 This application proposes the demolition of the existing two properties and the redevelopment of the site through the erection of a three storey block, plus accommodation within the mansard roof, to provide a total of 9 flats (7 x 2 bed and 2 x 4 bed). The building would be positioned to respect the building line to both Waverley Road and Crescent Road. Balconies are proposed to all flats and these are sited to the front and rear of the proposed block. Basement car parking is proposed, accessed from Crescent Road. Provision is made for 11 car parking spaces and 11 cycle parking spaces. The basement area also accommodates storage areas for each flat. Lift access would be provided from the basement parking area to all floors.

3 Relevant Planning Decisions

- 3.1 TP/96/0513 Planning permission granted for the redevelopment of land adjacent to 1 Crescent Road in December 1996 by the erection of a block of 7 two-bed flats, together with associated car parking spaces and front and rear dormer windows, now known as Glenview Lodge.
- 3.2 TP/91/1150 Planning permission granted for the change of use of part of No.1 Crescent Road house to Montessori Nursery/Playgroup for 16 children aged 2 1/2 - 5 year in December 1991. This permission was subsequently varied under reference TP/94/0763 to increase the number of children to 20. This use no longer appears to be evident at the site.

4 Consultations

4.1 Statutory and non statutory consultees

4.1.1 Traffic and Transportation note that:

- Crescent Road and Waverley Road are both local access roads.
- Double yellow lines apply to junctions with Waverley Road and Vermont Close, Waverley Road and Crescent Road, Haselwood Drive and Crescent Road.
- The location has poor public transport accessibility (PTAL 2)
- 12 cycle parking spaces and 11 car parking spaces (1 disabled) are proposed in the basement car park which equates to 1.2 spaces per flat in a secure underground car park, plus 2 visitor spaces on the forecourt.
- Ramp fall of 1:15 and headroom of 2.10 are both within the adopted standards.
- The car parking arrangement provides sufficient turning space.

- Cycle storage is to be located in the underground car park however a condition needs to be attached to make sure it is secure.
- The bin enclosure will not impede pedestrian sight line and will have a dropped kerb access.
- A single 5.6m wide crossover off Crescent Road is proposed. This is well over the allowed maximum of 4.9m for a single crossover and therefore amendments will be required and this can be addressed through condition.
- 2 pedestrian accesses (both 1.78m wide) off Crescent Road and Waverely Road are proposed.
- Two existing crossovers one to the junction with Waverely Road and another one on Waverely Rd will need to be closed up and the footway reinstated.

They raise no objection to the development and consider that it would not give rise to conditions prejudicial to the free flow or safety of traffic.

- 4.1.2 Education advise that the development would produce an average of 1 additional primary aged pupil a year equating to a contribution of £13,115. Secondary yield is negligible. This contribution will need to be secured through a S106 Agreement.

4.2 Public

- 4.2.1 Consultation letters have been sent to the occupiers of 87 nearby properties. In addition, the application has been advertised on site. In response, 24 letters of objection have been received which raise all or some of the following points:

- object to any more blocks of flats be built which is changing the character of the area
 - overdevelopment
 - this is no longer a brownfield site
 - nothing wrong with the existing properties
 - the road is already hazardous for parking and driving
 - increase congestion
- new traffic island at Old Park Road junction has increased traffic movements on Crescent and Waverley Road
 - access to basement car park close to a dangerous corner
- increasing damage to pavements from construction vehicles, delivery vehicles etc
- overloading utility infrastructure – gas, electricity , water and sewerage supplies
 - loss of light and outlook
 - loss of privacy
 - noise pollution
 - impact on trees
- the area is already overcrowded with the number of flat developments that have taken place
- the buildings would be higher than the existing buildings resulting in a loss of view
- could impact access for ambulances etc to the nearby elderly persons home.
- Noise, dust and disturbance

- Excavation for a basement car park could damage foundations of adjoining buildings
 - Impede access for maintenance and decoration of adjoining blocks
- 4.2.2 Councillor Vince, (Ward Councillor) and David Burrowed MP has also raised objections in support of their constituents concerns.

5 Relevant Policy

5.1 LDF – Core Strategy

- 5.1.1 At the meeting of the full Council on 10th November 2010, the Core Strategy of the Local Development Framework was approved. The document and the policies contained therein are now material considerations to be taken into account when considering the acceptability of development proposals. The following are of relevance

Core Policy 2	Housing supply and locations for new homes
Core Policy 4	Housing Quality
Core Policy 5	Housing Types
Core Policy 20	Sustainable Energy Use and Energy Infrastructure
Core Policy 21	Delivering sustainable water supply, drainage and sewerage infrastructure
Core Policy 30	Maintaining and improving the quality of the built and open environment
Core Policy 46	Infrastructure contributions

5.2 Unitary Development Plan

After the adoption of the Core Strategy, a number of UDP policies are retained as material considerations pending the emergence of new and updates policies and development standards within the Development Management Document. The following are of relevance

(II)GD3	Design and character
(II)GD6	Traffic implications
(II)GD8	Access and servicing
(II)H8	Privacy and overlooking
(II)H9	Amenity space
(II)T13	Access onto the public highway

5.3 London Plan

- 3A.1 Increasing London's Housing Supply
- 3A.2 Borough Housing targets
- 3A.3 Maximising the potential of sites
- 3A.5 Housing choice
- 3A.6 Quality of new housing provision
- 3C.23 Parking strategy
- 3D.13 Children and Young People's Play and informal recreation strategies
- 3D.14 Biodiversity and nature conservation
- 4A.3 Sustainable design and construction
- 4A.14 Sustainable drainage
- 4B.1 Design principles for a compact city
- 4B.5 Creating an inclusive environment

4B.8 Respect local context and communities

5.4 Other Material Considerations

PPS1	Delivering sustainable development
PPS3	Housing (June 2010)
PPG13	Transport

6 Analysis

6.1 Principle

6.1.1 The existing houses are not listed, nor are they located within a conservation area. Accordingly, planning permission is not required for their demolition.

6.1.2 The recent changes to PPS3 explicitly remove sites such as this from the definition of 'previously-developed land' and therefore the policy presumption in favour of making a more effective and efficient use of such land does not now apply. However, the changes within the PPS do not introduce an objection in principle to the redevelopment of such sites but the Council must continue to consider the application on its merits having regard to the impact of redevelopment on the character of the area, the amenities of the occupiers of adjoining properties, highway safety etc. However as the immediately surrounding area is entirely residential in character and in the main consists of flatted developments, the use of this site for more intensive residential purpose is considered acceptable in principle and consistent with the character of the area.

6.2 Impact on the character of the area

6.2.1 The immediately surrounding area in the main consists of flatted developments and the proposed development of flats would be consistent with this.

6.2.2 The character of the surrounding area has both urban and suburban characteristics and therefore the London Plan would suggest a wide density range of from 150 to potentially 450hrph. However, given the PTAL rating of 2 a density mid-range would be appropriate. The site has an area of 0.1017 hectares. This application proposes 33 habitable rooms, giving a density of 324 hrph.

6.2.3 This numerical assessment of density should also be taken with an assessment of the size and scale of the building proposed and how it sits within the context of the area. The area is dominated by flatted developments, the majority of which are of a size and scale consistent with the building proposed. Accordingly, the density of development proposed and the scale of building necessary to achieve this is considered acceptable and consistent with the character of the area.

6.2.4 The Council's standards require that amenity space provision should equate to 75% of the gross internal area of the proposed building. The application makes provision for 61% and therefore is below the Council's standards. This level of amenity space provision is not dissimilar to the level of amenity space that supports adjacent blocks of flats. Notwithstanding this, to address the fact that the development does not comply with standards and to address the fact

that future residents would need to make use of existing areas of open space to meet their active recreational needs, the applicant has agreed to a contribution of £30,000 towards the enhancement of existing facilities or access thereto. This would be secured through a S106 Agreement.

6.2.5 In addition, each flat at first floor level and above would be provided with at least two good sized usable balconies with the larger 4 bed units having the benefit of larger balconies to meet the passive needs of residents and provide access to some outside space. The ground level amenity space is largely provided to the front of the proposed building and ensures a setting consistent with the character of the area.

6.2.6 The building is designed to reflect the prevailing character of the area in terms of its elevational treatment and this is considered acceptable.

6.3 Housing Mix

6.3.1 In terms of variety of housing mix, the development of 9 units is relatively modest and therefore it would be difficult to achieve complete compliance with the Council's preferred housing mix, as set in Core Policy 5. However, the Strategic Housing Market Assessment that provided the evidence base for this policy identifies the greatest need in market housing to be for 3 bed + units. This application includes provision for 2 x 4 bed units. Although these are located on the upper floors, they are large units which would provide good sized family accommodation with access to balconies and or roof top amenity space.

6.4 Access, traffic and parking

6.4.1 The access arrangements into the site are considered acceptable, with appropriate visibility and an acceptable gradient to the basement car park. The level of parking proposed at 1.3 spaces per unit is in accordance with London Plan standards. In addition, 2 visitor spaces are proposed at surface level. Notwithstanding the objections raised by local residents on traffic, access and parking grounds, it is considered that the development would not give rise to conditions prejudicial to the free flow and safety of vehicles using the adjoining highways.

6.5 Sustainable Design and Construction

6.5.1 The development is to achieve a Code 3 for Sustainable Homes, in line with Core Policy 4. A condition is recommended to secure this and require the submission of the necessary certification.

6.5.2 The development presently does not achieve all Lifetime Homes Standards. However, this can be resolved with amendments to the internal layout and a condition is recommended requiring this to be undertaken to ensure compliance as required by London Plan policy 3A.5 and Core Policy 4.

6.6 Impact on trees

6.6.1 In order to achieve appropriate disabled access to the building, the development requires the removal of the Sweet Chestnut to the site frontage, the subject of a Tree Preservation Order. Moreover, it should be noted that

consistent with adjoining developments, the site level is considerably above pavement level.

6.6.2 The Arboricultural Report submitted with the applicant confirms that the tree is growing out of a raised bed and this now has a full height crack in the retaining wall adjacent to the footway. The tree has evidence of bark splitting, cracking and flaking in the trunk. The Tree Officer supports the removal of the tree subject to a replacement being secured through a condition.

6.7 Impact on adjoining residents

6.7.1 No.31 Waverley Road (Willowside Court) to the north of the site comprises a three storey block of flats, containing 6 windows in its flank elevation facing the application site; two windows to each floor serving bathrooms and kitchens. The conditions of the planning permission for the Willowside Court development required that these windows be obscure glazed.

6.7.2 The existing chalet bungalow on the application site is sited on the common boundary with Willowside Court and this has an impact on the flank windows to the ground and first floor flats. The proposed development would introduce a three storey elevation but at a distance of 2.3m from the flank wall of Willowside Court. The impact of this on the ground and first floor flats is likely to be similar to the existing situation. The 2nd floor flat presently stands above the height of the bungalow roof and therefore is not unduly affected by it. The proposed development would certainly have a greater impact. However, given the position of the windows in the flank in relation to the proposed building, which step in considerably from the boundary at a point just past the first window and given these windows serve non-habitable accommodation and are obscure glazed, the development is considered acceptable in terms of its impact on the amenities of the occupiers of this property in terms of light and outlook. No windows are proposed in the flank elevation of the building adjacent to Willowside Court and therefore the development would not give rise to any loss of privacy.

6.7.3 The adjacent block to the Crescent Road frontage, Glenview Lodge, is a ¾ storey block of flats with no windows in the flank elevation. The development would not have any undue impact on the amenities of the occupiers of this block in terms of light or outlook. The development does include provision for windows and balconies on the rear elevation facing the rear garden of Glenview Lodge. However, given this is a communal garden, and given a separation distance of approximately 11m is achieved, it is considered that the development would not result in any undue loss of privacy.

6.7.4 The flatted developments opposite the application site are separated by the existing roads. Given this and the position of the proposed block, respecting existing building lines, it is considered that the development would not have an undue impact on the amenities of the occupiers of these blocks.

6.7.5 Concerns have been raised by nearby residents about noise and disturbance. The development of the site for flats is not likely to give rise to undue noise and disturbance once completed. An element of noise and disturbance is inevitable during the construction process but where unreasonable can be addressed through other statutory controls.

6.7.6 Concern about the impact of the development on the foundations to adjoining blocks would be addressed through Buildings Regulations and/or The Party Wall Act.

6.7.7 Issues regarding access for future maintenance are not material to the consideration of this application.

6.8 S106 Agreement

6.8.1 A S106 Agreement is required to secure the education and open space/access contributions referred to above.

7 **Conclusion**

7.1 In conclusion the development of the site as proposed is considered acceptable in the context of the area and having regard to the amenities of the occupiers of adjoining and nearby properties. Accordingly, it is recommended that planning permission be granted for the following reasons:

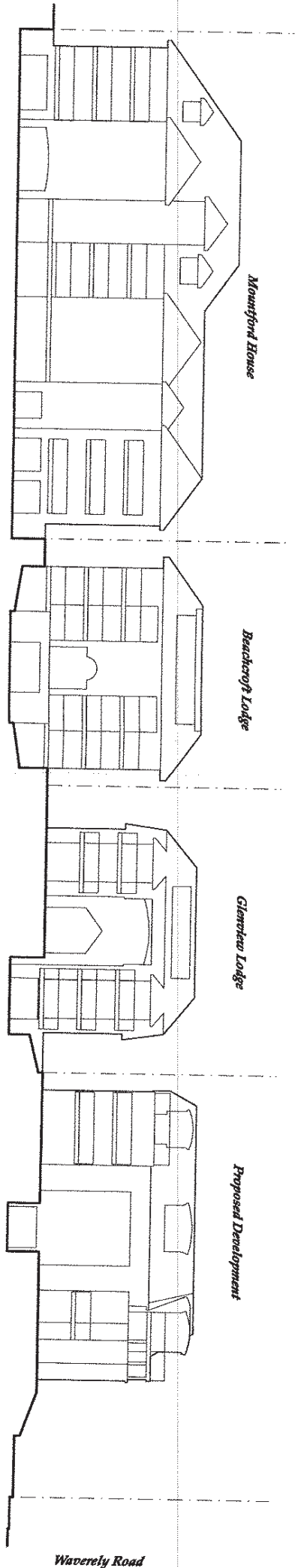
- 1 The proposed development has appropriate regard to the character of the area and the amenities of the occupiers of adjoining and nearby properties. In this respect the development complies with Core Strategy policy 30, Unitary Development Plan policies (II)GD3 and (II)H8 and London Plan policies 4B.1 and 4B.8.
- 2 The development makes appropriate provision for car parking and will not give rise to conditions prejudicial to the free flow and safety of traffic using the adjoining highway, including pedestrian traffic. In this respect the development complies with Policies (II)GD6, (II)GD8 and (II)T13 of the Unitary Development Plan and London Plan policy 3C.23

8 **Recommendation:**

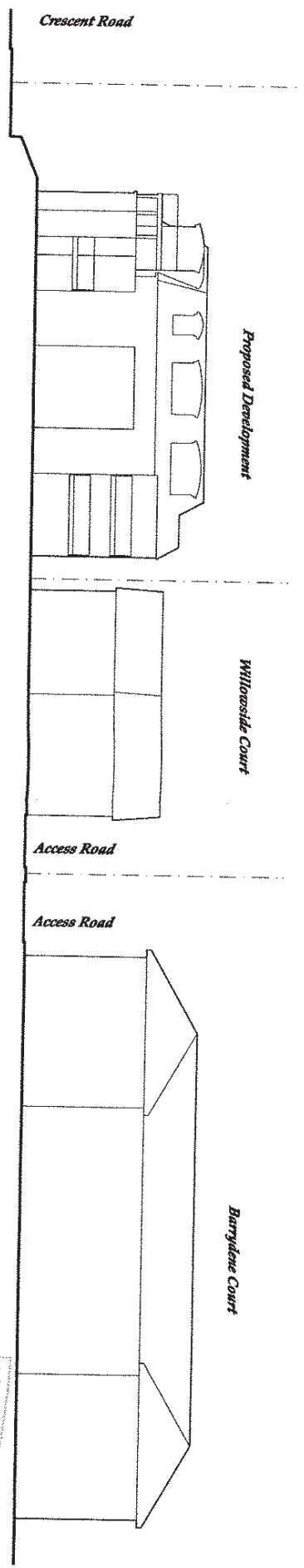
8.1 That subject to the satisfactory completion of a S106 Agreement to secure the contribution referred to in this report, planning permission be GRANTED subject to the following conditions:

- 1 C07 Details of materials
- 2 C09 Details of hard surfacing
- 3 C11 Details of enclosure
- 4 C14 Details of access and junction
- 5 C16 Private vehicles only – parking areas
- 6 C17 Details of landscaping
- 7 C18 Details of tree protection
- 8 C19 Details of refuse storage
- 9 C22 Details of wheel cleaning
- 10 Before the development hereby permitted commences an initial design stage assessment shall be carried out by an accredited assessor for the Code for Sustainable Homes and an interim certificate confirming compliance with at least level 3 of the Code shall be submitted to and acknowledged in writing by the Local Planning Authority. The dwellings shall not be occupied until a final Code certificate of compliance has been issued.
Reason: To ensure that the development is built in accordance with the Code for Sustainable Homes

- 11 Notwithstanding the floor plans submitted, development shall not commence until floor plans have been submitted to and approved by the Local Planning Authority that demonstrate compliance with the Lifetime Homes Standards. The development shall be completed in accordance with the approved plans prior to occupation.
Reason: In order to ensure compliance with Core Strategy Policy 4 and London Plan policy 3A.5.
- 12 C59 Cycle parking
- 13 Notwithstanding the drawings submitted, the proposed vehicle access to the basement car park shall not exceed 4.9m in width.
Reason: In the interests of highway safety.
- 14 The development shall not be occupied until the existing redundant points of access to the site have been closed and the footway reinstated.
Reason: To confine vehicles movements to permitted points of access, to enable additional kerbside parking to the roadway and to improve the condition of the adjacent footway.
- 15 C51a Time Limited Permission



STREET VIEW FROM CRESCENT ROAD



STREET VIEW FROM WAVERLEY ROAD

LONDON BOROUGH OF ENFIELD
 13 SEP 2010
 02:00:00

Revisions

A 3/10 Step introduced into roof profile on Waverley Road elevation

NETWORK
 design

ARCHITECTS
 interior design
 project managers
 surveyors
 structural eng
 services eng
 town planners

1a sinah lane · Hayling island
 Hampshire · PO11 0EY
 t: 02392-365887
 e: network.design@nlworld.com

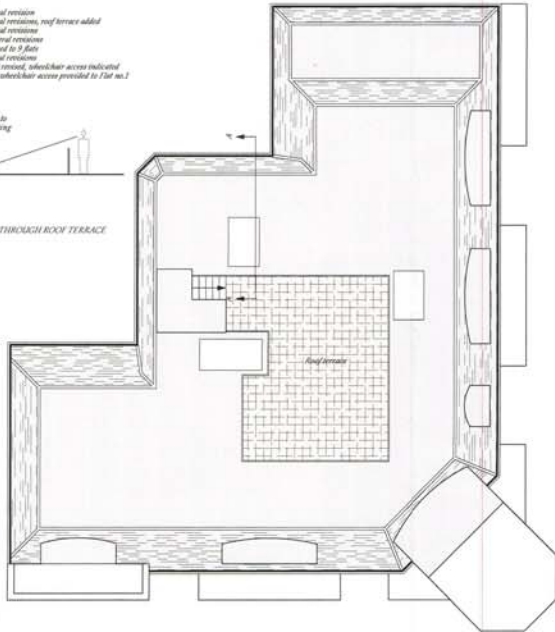
project: 1 Crescent Road, Enfield
 drawing: Sheet Views
 scale: 1:300 date: March 2010

drawing no: AN/2007/0002

Revisions
 A 4/09 Central revision
 B 5/09 Central revision, roof terrace added
 C 7/09 Central revision
 D 10/09 Central revision
 E 1/10 Reduced to 9 flats
 F 3/10 Central revision
 G 8/10 Full 9 revised, wheelchair access indicated
 H 10/10 Full wheelchair access provided to Flat no.1

Terrace set back to prevent overlooking

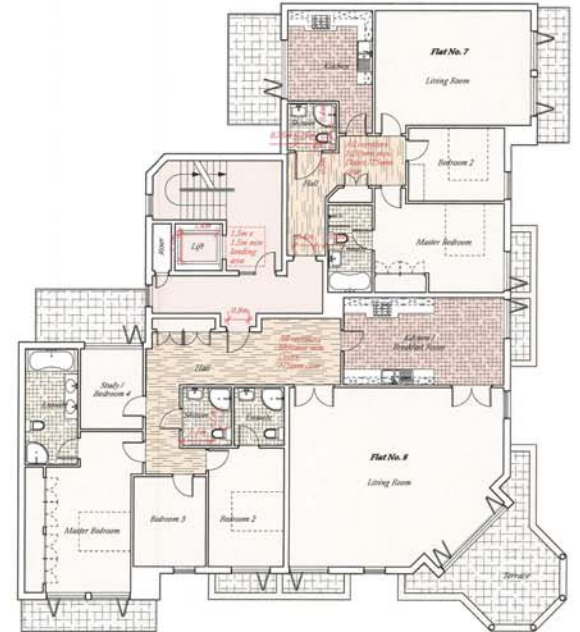
SECTION A-A THROUGH ROOF TERRACE



ROOF PLAN



PENTHOUSE



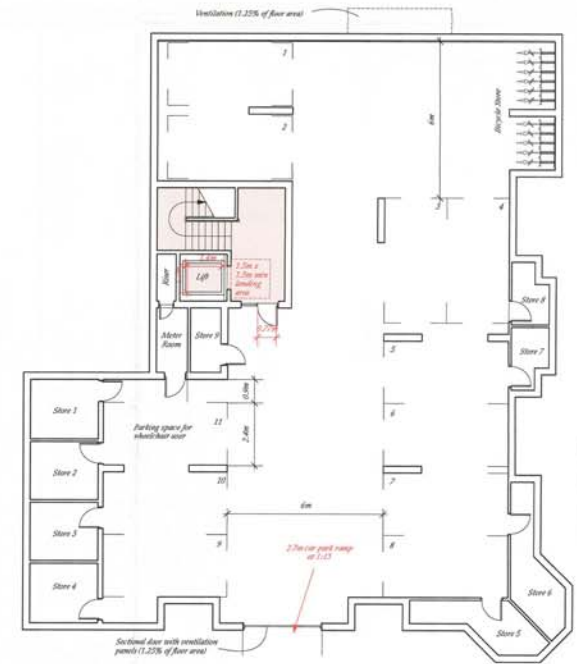
SECOND FLOOR



FIRST FLOOR



GROUND FLOOR



project: 1 Crocett Road, Enfield EN2 7BN
 drawing: Floor Plans
 scale: 1:100 date: December 2008
 drawing no: M/29508/002 rev: H

© NETWORK design
 ARCHITECTURE DESIGN PROJECT MANAGERS
 14 SILK LANE, BAYLIS ISLAND, HAMMERSLEY, PO11 8JF
 TEL: 02392-354882 FAX: 02392-342887